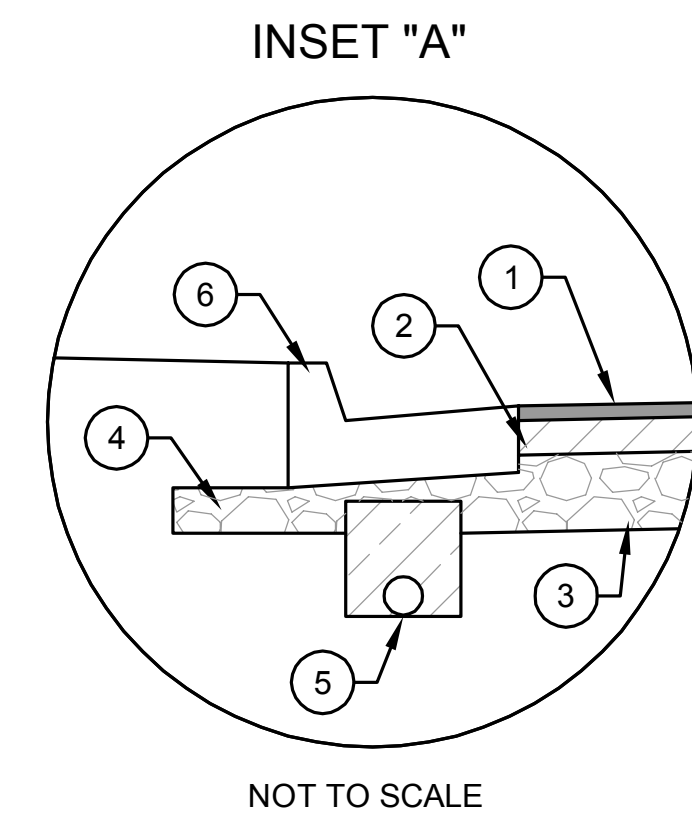
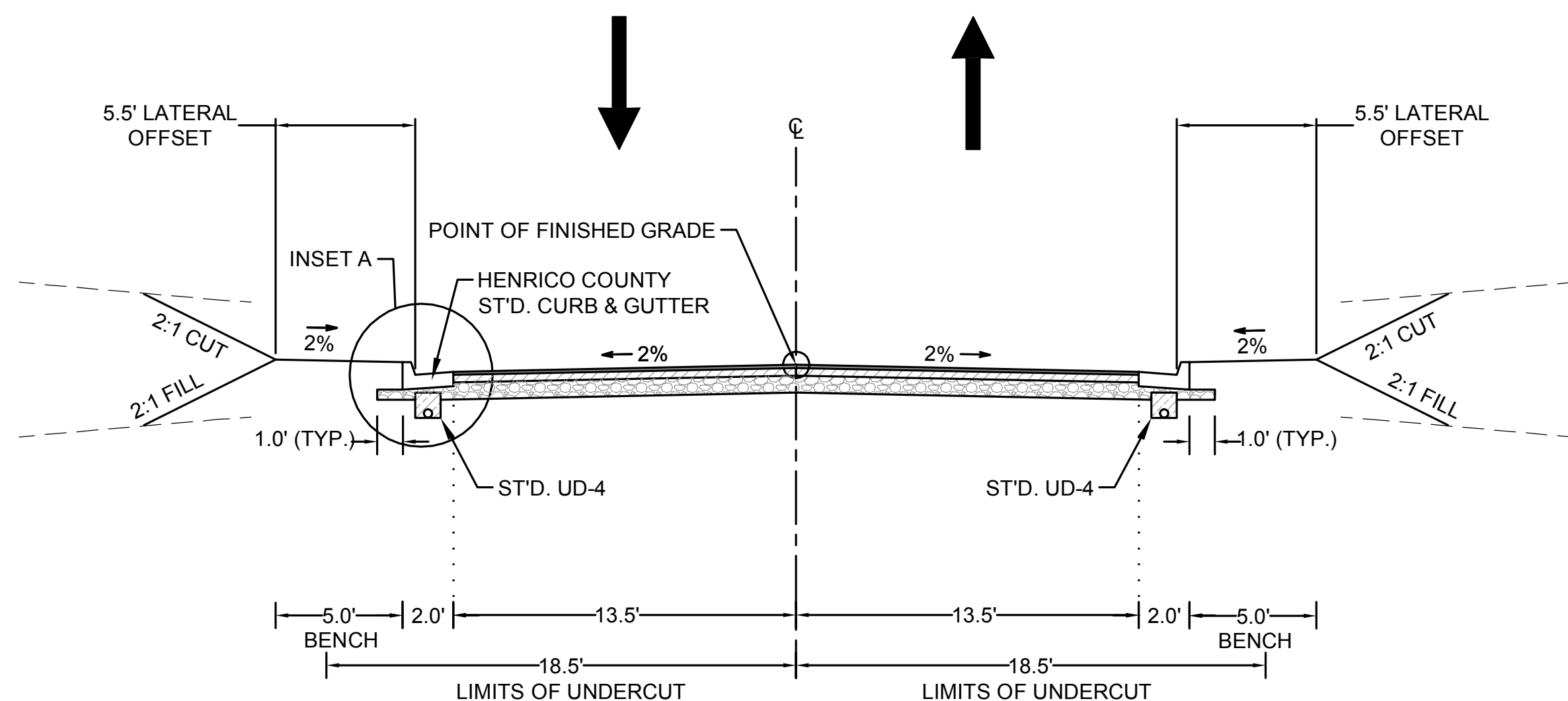


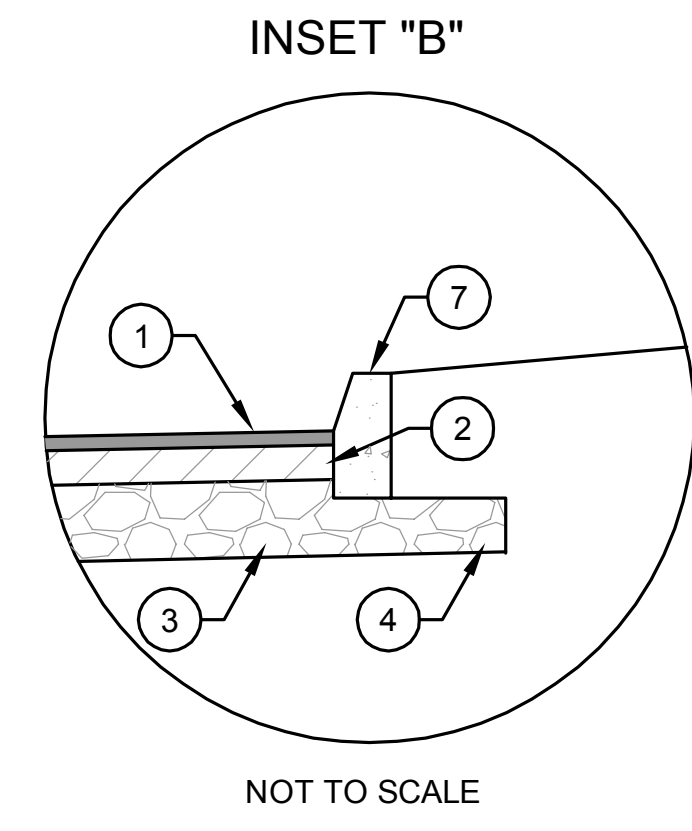


DOMINION ACCESS ROAD TYPICAL SECTION (STREET WITH CURB & GUTTER DESIGN)

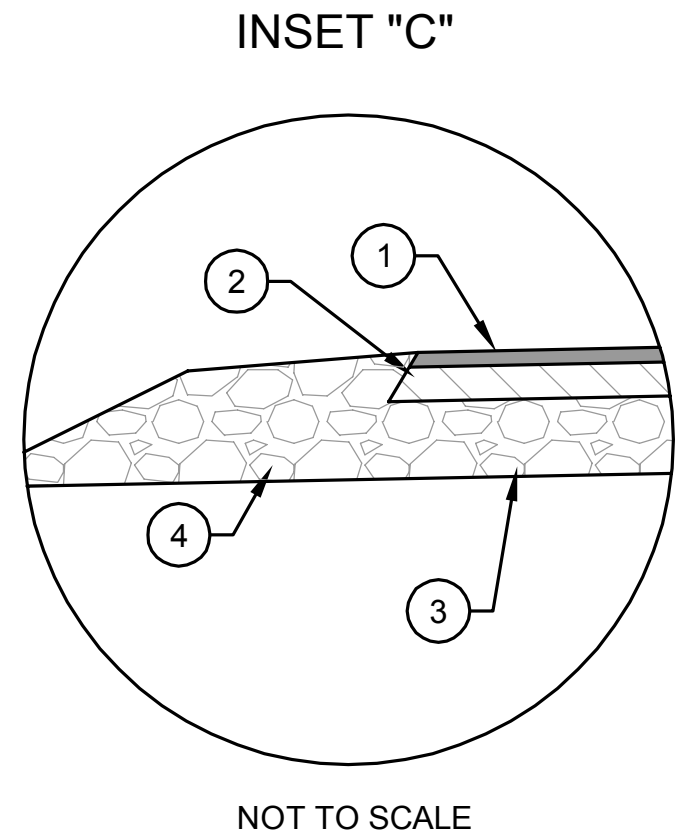
STA. 10+24.61 TO STA. 16+21.89



NOT TO SCALE



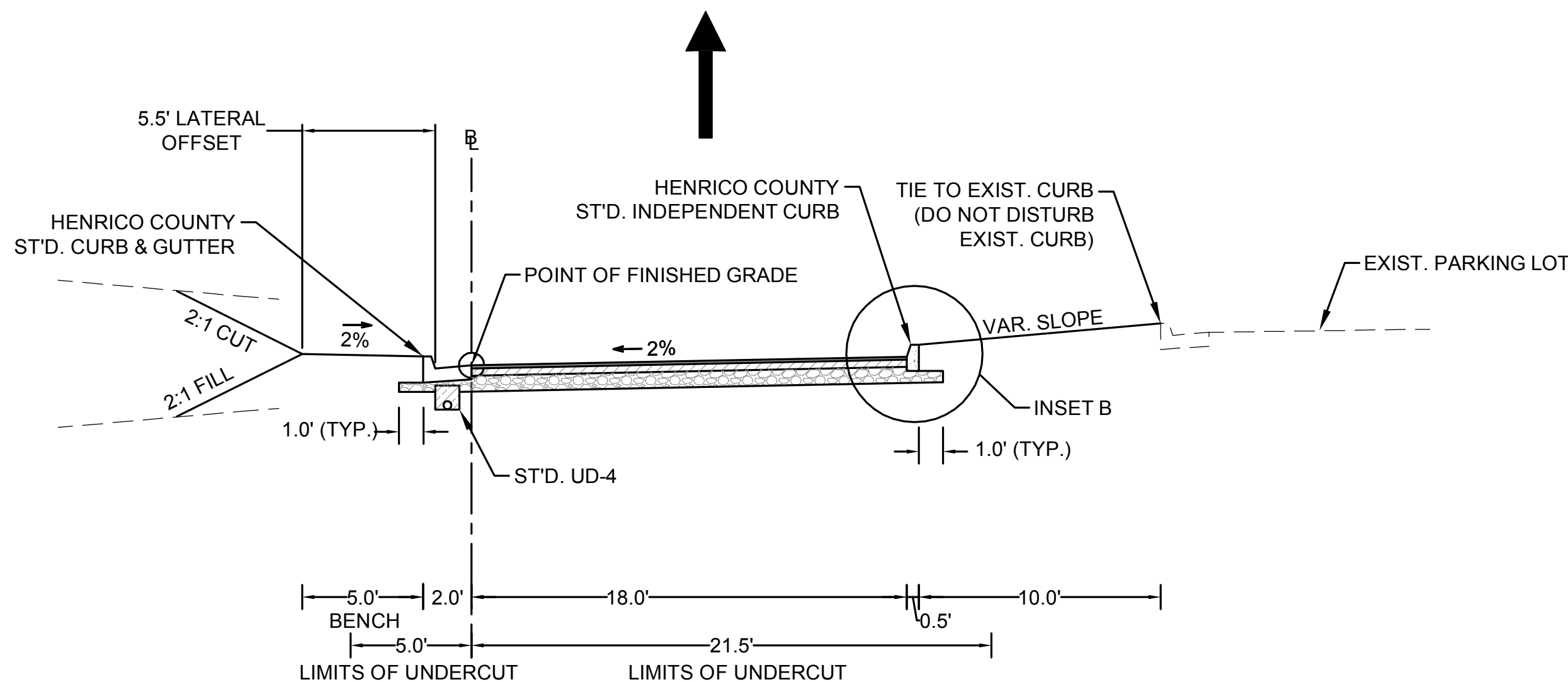
NOT TO SCALE



NOT TO SCALE

TURNAROUND ROAD TYPICAL SECTION (STREET WITH CURB & GUTTER DESIGN)

STA. 20+00.00 TO STA. 23+00.61

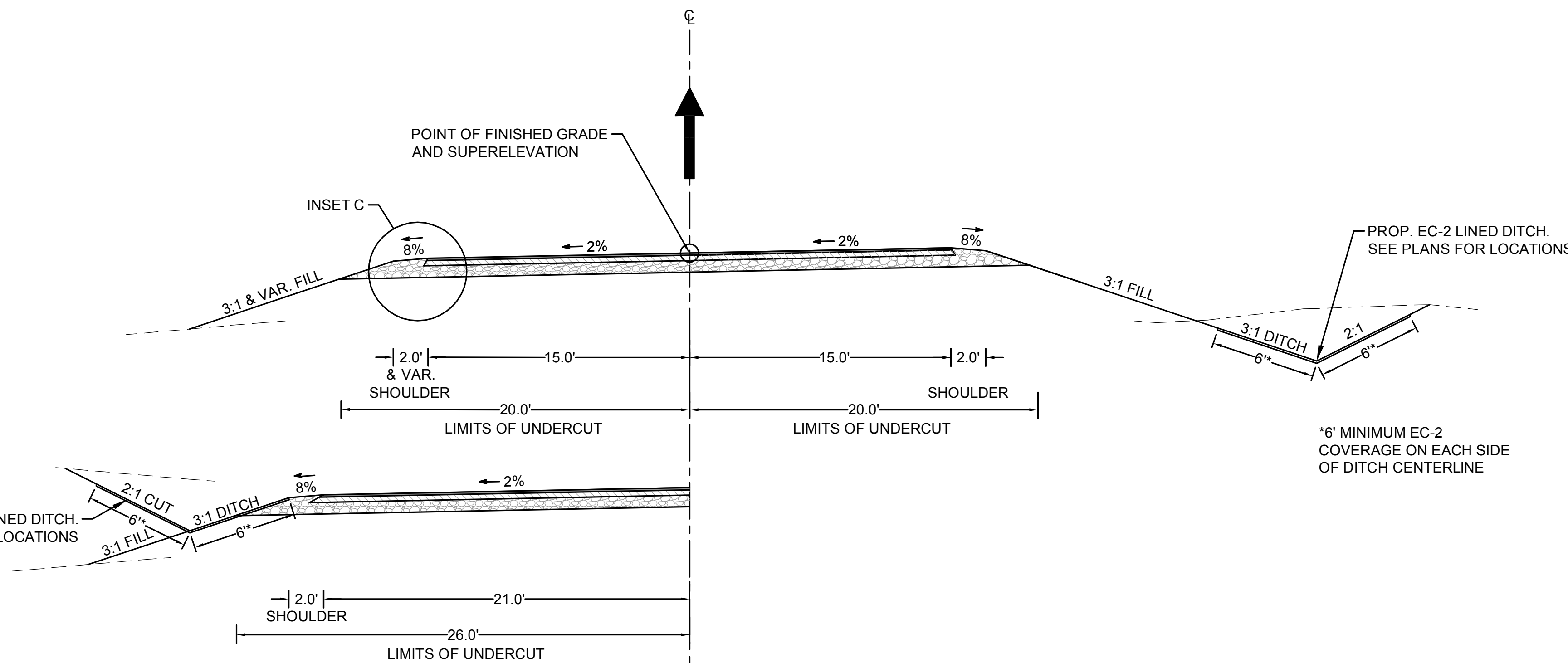


FUEL FARM ROAD TYPICAL SECTION (STREET WITH SHOULDER DESIGN)

30' PAVEMENT WIDTH
STA. 32+43.43 TO STA. 35+40.70
SEE SHEETS 4, XS-04, AND XS-05 FOR GRADING DETAILS OF FUEL FARM ROAD INTERIOR

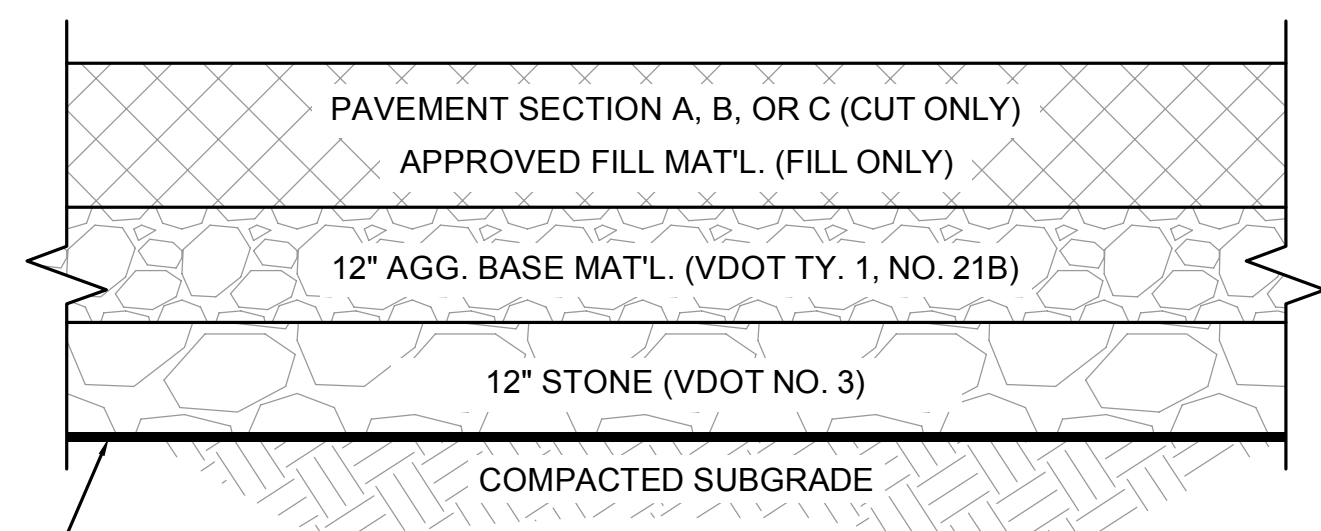
36' PAVEMENT WIDTH
STA. 30+34.80 TO STA. 32+43.43

*6' MINIMUM EC-2 COVERAGE ON EACH SIDE OF DITCH CENTERLINE



- ① 1.5" ASPHALT CONCRETE SURFACE COURSE @ 165 LBS/SY (SM-9.5A)
- ② 3.5" ASPHALT CONCRETE BASE COURSE (BM-25.0A)
- ③ 8" AGGREGATE BASE MATERIAL (TYPE 1 NO. 21B)
- ④ VAR. DEPTH AGGREGATE BASE MATERIAL (VDOT TYPE 1, NO. 21B)
- ⑤ VDOT UD-4 UNDERDRAIN
- ⑥ HENRICO COUNTY ST'D. CURB AND GUTTER
- ⑦ HENRICO COUNTY ST'D. INDEPENDENT CURB

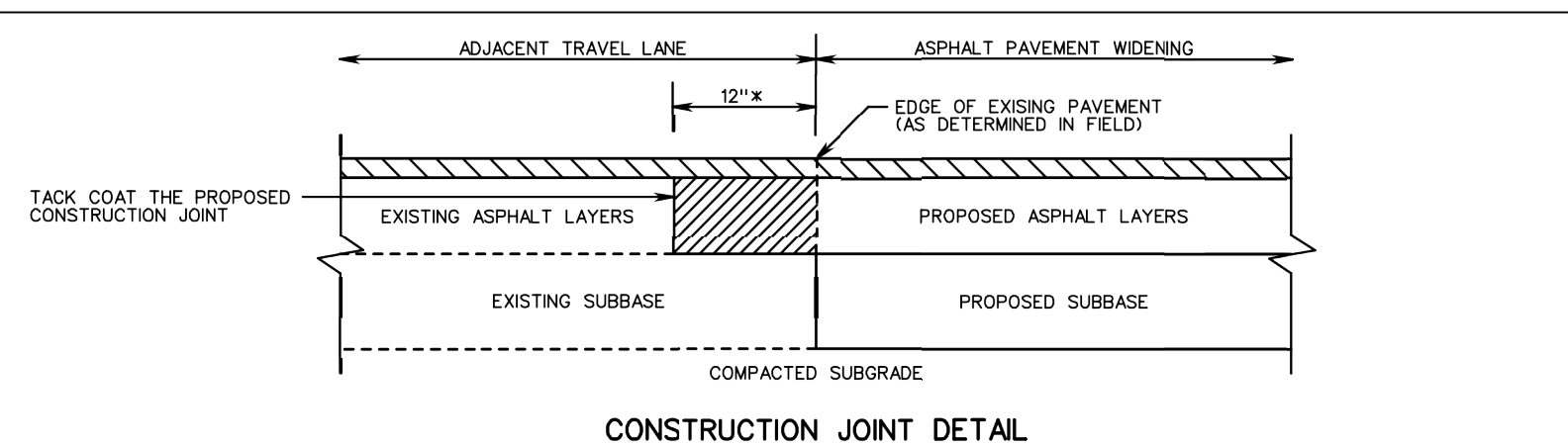
UNDERCUT SECTION TO BE PERFORMED UNDER ALL ROADWAY SECTIONS UNLESS OTHERWISE DIRECTED BY THE ENGINEER



NOTE: AGGREGATE AND STONE SHALL BE COMPACTED IN ACCORDANCE WITH VDOT SPECIFICATIONS. TENSAR BX1300 OR APPROVED SIMILAR GEOGRID SOIL STABILIZATION SHALL BE PLACED UNDERNEATH NO. 3 STONE SECTION.

NOTE: UNDERCUT SECTION SHOWN ABOVE SHALL BE APPLIED UNDER ROADWAY SECTIONS AS DEPICTED ON TYPICAL SECTIONS. THIS SECTION SHALL ALSO BE APPLIED UNDERNEATH GRADED AREA FOR FUTURE FUEL FARM SUPPORT BUILDINGS AS SHOWN ON SHEETS 4 AND XS-04.

THE NEED FOR UNDERCUT IN OTHER AREAS SHALL BE FIELD-DETERMINED BY THE GEOTECHNICAL ENGINEER.



- REMOVE EXISTING ASPHALT LAYERS TO EXISTING SUBBASE AND REPLACE WITH PROPOSED ASPHALT WIDENING LAYERS
- PROPOSED MINIMUM 1.5 INCH THICK ASPHALT SURFACE COURSE (SEE NOTE 5)
- MINIMUM 12 INCHES, OR GREATER AS NECESSARY TO ABUT THE FULL THICKNESS OF EXISTING ASPHALT LAYERS AS DETERMINED BY CORES (SEE NOTE 3)

- NOTES:
- ASPHALT PAVEMENT WIDENING SHALL HAVE A PAVEMENT DESIGN IN ACCORDANCE WITH CURRENT VDOT PROCEDURES AND BE APPROVED BY THE ENGINEER.
 - THE PAVEMENT DESIGN FOR ASPHALT PAVEMENT WIDENING SHALL MEET OR EXCEED THE DEPTHS AND TYPES OF THE LAYERS OF EXISTING PAVEMENT. SUBSURFACE DRAINAGE OF THE EXISTING AND PROPOSED PAVEMENT SHALL BE ADDRESSED IN THE PAVEMENT DESIGN.
 - A MINIMUM OF THREE CORES SHALL BE TAKEN ALONG THE CENTER OF THE ADJACENT TRAVEL LANE TO DETERMINE THE TYPE AND THICKNESS OF EXISTING PAVEMENT LAYERS. THESE CORES SHALL BE SPACED NO MORE THAN 500 FEET APART.
 - THE ADJACENT TRAVEL LANE SHALL BE MILLED A MINIMUM DEPTH OF 1 1/2 INCHES AND REPLACED WITH AN ASPHALT SURFACE COURSE TO MATCH THE PROPOSED PAVEMENT WIDENING SURFACE COURSE, UNLESS WAIVED BY THE ENGINEER.
 - THE ENGINEER MAY REQUIRE THE MILLING DEPTH OF THE EXISTING PAVEMENT TO BE ADJUSTED TO ACHIEVE AN ACCEPTABLE PAVEMENT CROSS-SLOPE AND EFFECTIVE SURFACE DRAINAGE.
 - EXISTING PAVEMENT MARKINGS AND MARKERS WITHIN THE PROJECT LIMITS SHALL BE RESTORED SUBJECT TO THE APPROVAL OF THE ENGINEER.
 - FINAL TRANSVERSE PAVEMENT TIE-IN SHALL CONFORM TO THE REQUIREMENTS OF SECTION 315.05(c) OF THE SPECIFICATIONS EXCEPT THAT ALL JOINTS AT TIE-IN LOCATIONS SHALL BE TESTED USING A 10 FOOT STRAIGHTEDGE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 315.07(d) OF THE SPECIFICATIONS.

VDOT PAVEMENT WIDENING DETAIL (ST'D. WP-2) TO BE FOLLOWED WHEREVER PROPOSED PAVEMENT TIES TO EXISTING PAVEMENT

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TIMMONS GROUP

Site Development | Residential | Infrastructure | Technology

EAST RAMP 3 ACCESS ROAD

HENRICO COUNTY - VIRGINIA

TYPICAL SECTIONS

YOUR VISION ACHIEVED THROUGH OURS.

THE DRAWING PREPARED AT THE
 CORPORAION'S OFFICE
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REVISION DESCRIPTION	DATE	DRAWN BY	DESIGNED BY	CHECKED BY	SCALE
	6/30/2019	KPO	KPO	CMK	N.T.S.

JOB NO.

40930.001

SHEET NO.

02A

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